

OWNED BY:	Darwin Port Corporation
OPERATED BY:	Multi-user
TYPE OF FACILITY:	Multi-user berth designed to handle livestock exports, dry bulk imports containerised, breakbulk, general and heavy lift cargoes.
ACCESS:	By road and rail.
WORKING HOURS:	24 Hours / Day

Berth Details

Type:	Sheet pile with retained fill
Length:	600m
Fenders:	svedala (Fender face 4m from wharf face)
Apron width:	25.3m + rear access roadway.
Depth alongside:	13m chart datum
Under keel clearance alongside:	0.5m
Height of deck level above chart datum:	9.4m
Deck surface:	Bituminous Cement Polymer Modified
Deck loading:	6t/sqm or 1000t over 10m x 10m
Alignment:	127°/307°
Approach:	2.5km causeway

Cranage

IHI/Sumitomo single-lift, rail mounted gantry crane.

Maximum outreach from fenders - 32m

Height from deck level to underside of boom - 30m

Lifting capacity: 40t @ 32m outreach

50t @ 23m outreach

70t @ 14.450m outreach

Equipped with a 10m³ grab for bulk cargoes.

100t – 150t Crawler Cranes on Berth.

Shipping

Design vessel capacity:	75,000 DWT
Beam:	Unlimited
LOA	Unlimited
Arrival draft:	12m + tide – 1.5m UKC
Departure draft:	12m + tide – 1.5m UKC
(Drafts in excess of 12m by approval of the Harbour Master)	

Common User Facilities

10 hectares of sealed hardstand.

8 hectares of bunded area for future reclamation.

4,000sqm cargo transit shed with dry bulk cargo handling facilities and 20 reefer outlets.

24 reefer outlets on hardstand.

Container washdown and pre-tripping facilities.

Quarantine & Customs Services.

Dry bulk cargo import facilities.

Quarantine waste collection and disposal.

Sewage and oily bilge receptacle tanks. (Pipeline from wharf face)

Freightlink - Intermodal Facilities

A 16m wide railway access embankment for the Adelaide to Darwin railway.

An Intermodal Container facility incorporating:

- Priority access to 220m of the existing common user berth facility
- A nominal 180m x 240m intermodal container terminal yard with 924 TEU ground slot capacity. The facility has two 200m rail sidings.

Bulk Liquids Berth

Provision of a dedicated bulk liquids berth by extending the general purpose berth by an additional 154m to provide a total of 754m continuous berth face plus mooring dolphins.

The following are the requirements of petroleum vessels that can use this facility:

Vessels up to 70,000 tonnes DWT

- Current depth of water alongside is 11m at Chart Datum. Vessels must maintain a 0.5m under keel clearance whilst alongside. Therefore, the maximum draft, unless otherwise approved by the Harbourmaster, is 10.5m.

All vessels must have a parallel side from the bow to the manifold, or the manifold it intends to use, of not less than 35.2m. This means that the vessel will starboard side to. If the vessel has a parallel side from the stern to the manifold, or manifold it intends to use, or not less than 35.2m, it will go to port side to. The vessel must confirm that they can maintain this minimum parallel side throughout the period it is secured alongside.

Should a vessel need only to use one Marine Loading Arm and this is the western most MLA the vessel must have the following parallel side:

A parallel side from the bow to the manifold to be used of 32.7m or from the stern to the manifold to be used of 32.7m. This will determine which side to the vessel parallel side throughout the period it is secured alongside.

Physical measurements of the berth have been taken which have required the original parameter for parallel side requirements to be amended.

For more details of the Vopak Darwin Terminal go to www.vopak.com

Master Plan

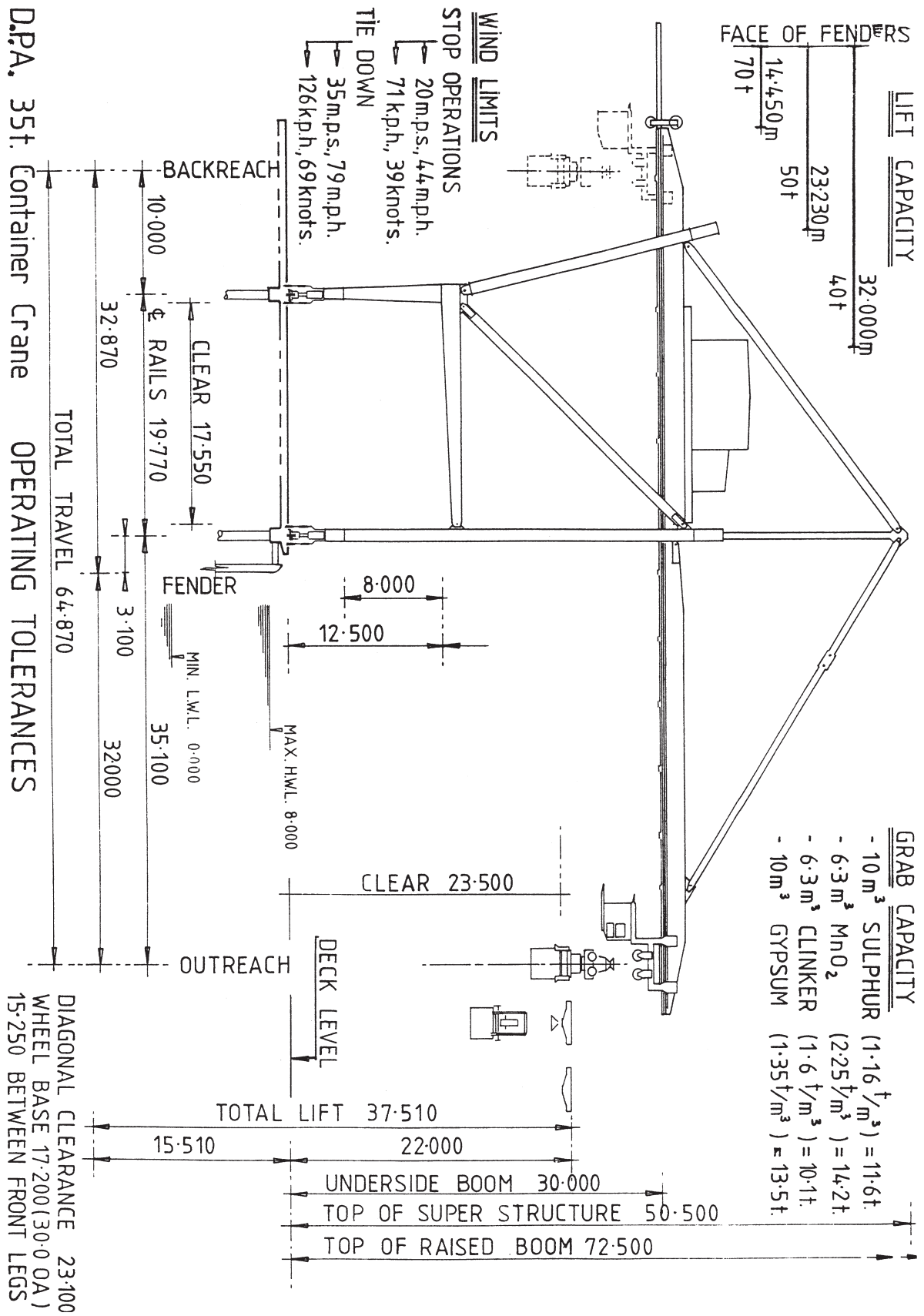
The master planning for the future development of East Arm nominates a second stage of development as outlined below:

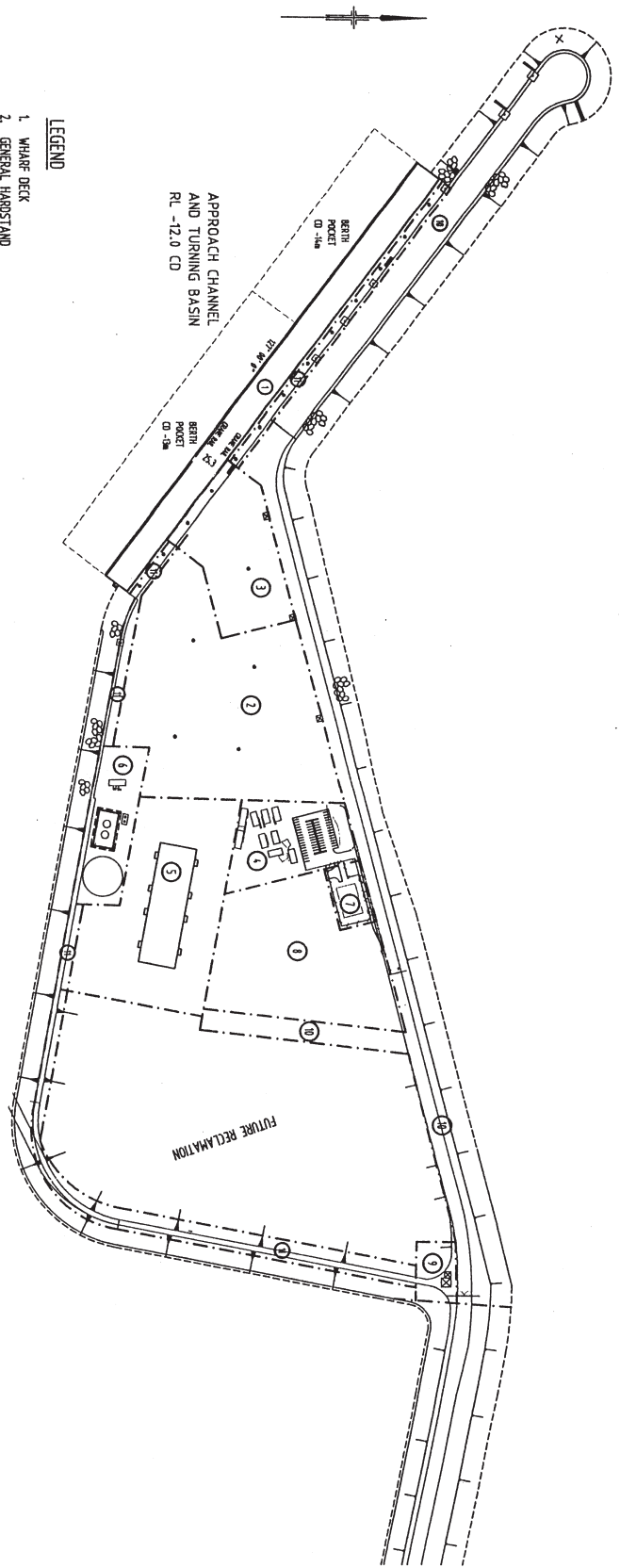
The master plan makes provision for in excess of 1.5km of continuous berth face.

The plan includes dedicated bulk liquid export berths, extension of the common user berth and a corresponding extension of the container storage yard and rail sidings

Further reclamation behind the general-purpose berths providing additional areas for general cargo handling.

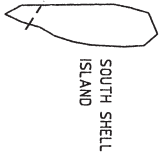
Provision of a dry bulk import/export facility by extending the general purpose berth to the East and the construction of travelling gantry shiploader/unloaders on the berth. Reclamation of a 10 hectare stockpile area East of the existing reclamation and the installation of dry bulk loading conveyor systems.





LEGEND

1. WHARF DECK
 2. GENERAL HARBOURSTAND
 3. HEAVY DUTY HARBOURSTAND
 4. OFFICES
 5. TRANSIT SHED & HARBOURSTAND
 6. WASTE TREATMENT & BULK LIQUIDS STORAGE
 7. TEMPORARY DIESEL STORAGE
 8. OFFSHORE SUPPLY BASE
 9. GATEHOUSE & SERVICES
 10. ACCESS CORRIDORS
 11. SERVICE CORRIDORS
- LIGHTING COLUMN
 - SUBSTATION
 - ORN DOLPHIN
 - BOLLARD DOLPHIN
 - CONTAINER POWER OUTLET










PROPOSED
BULK LIQUIDS/
SOLID FACILITY
(STAGE 2A)



EAST ARM PORT - STAGE 1
CURRENT LAND USE PLAN



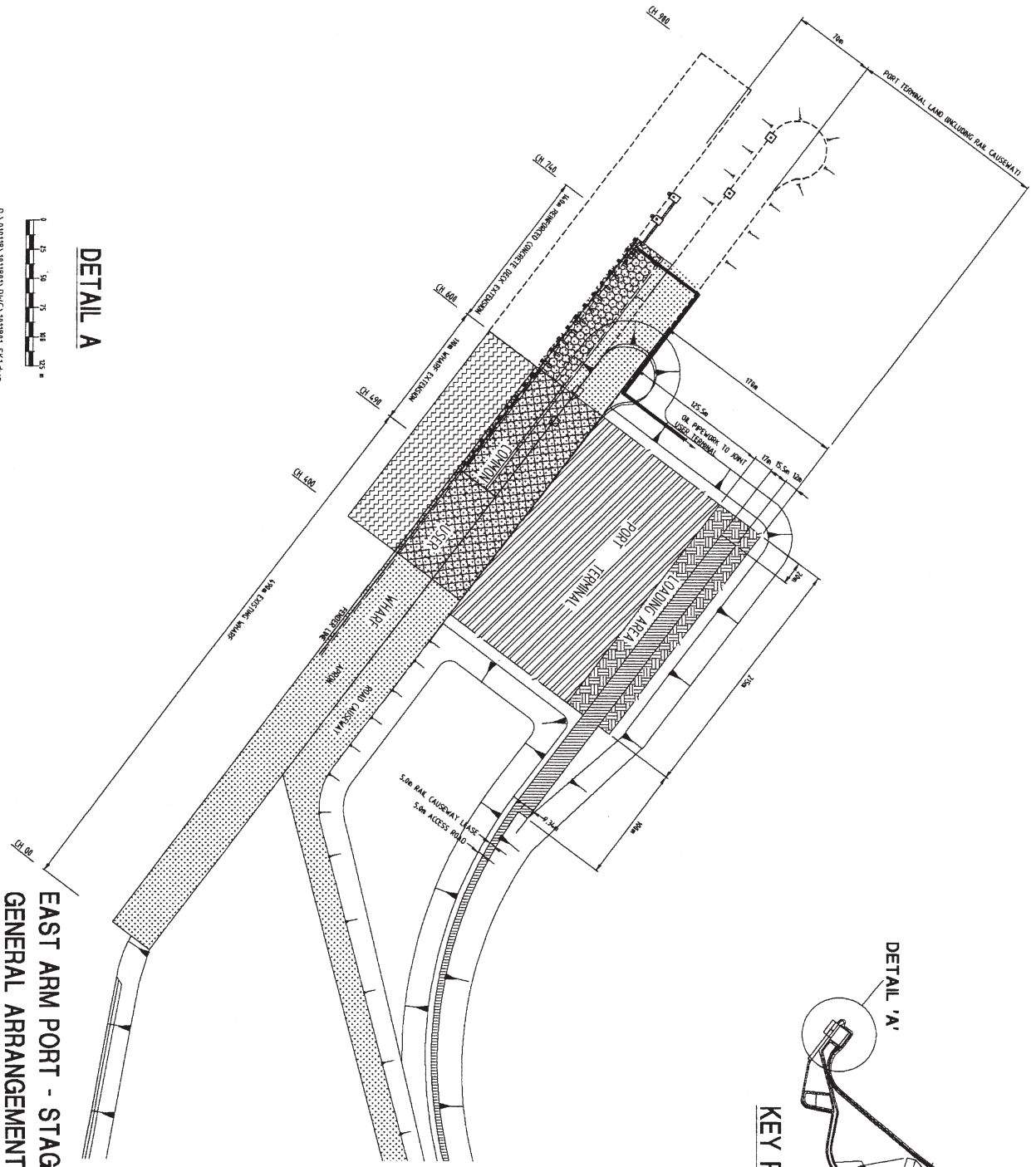
LEGEND

	LOADING AREA.
	COMMON USER WHARF.
	PRIORITY USER WHARF AREA.
	PORT TERMINAL.
	RAILWAY CAUSEWAY LEASE AREA.
	PRIORITY BERTH AREA.
	BULK LIQUIDS BERTH

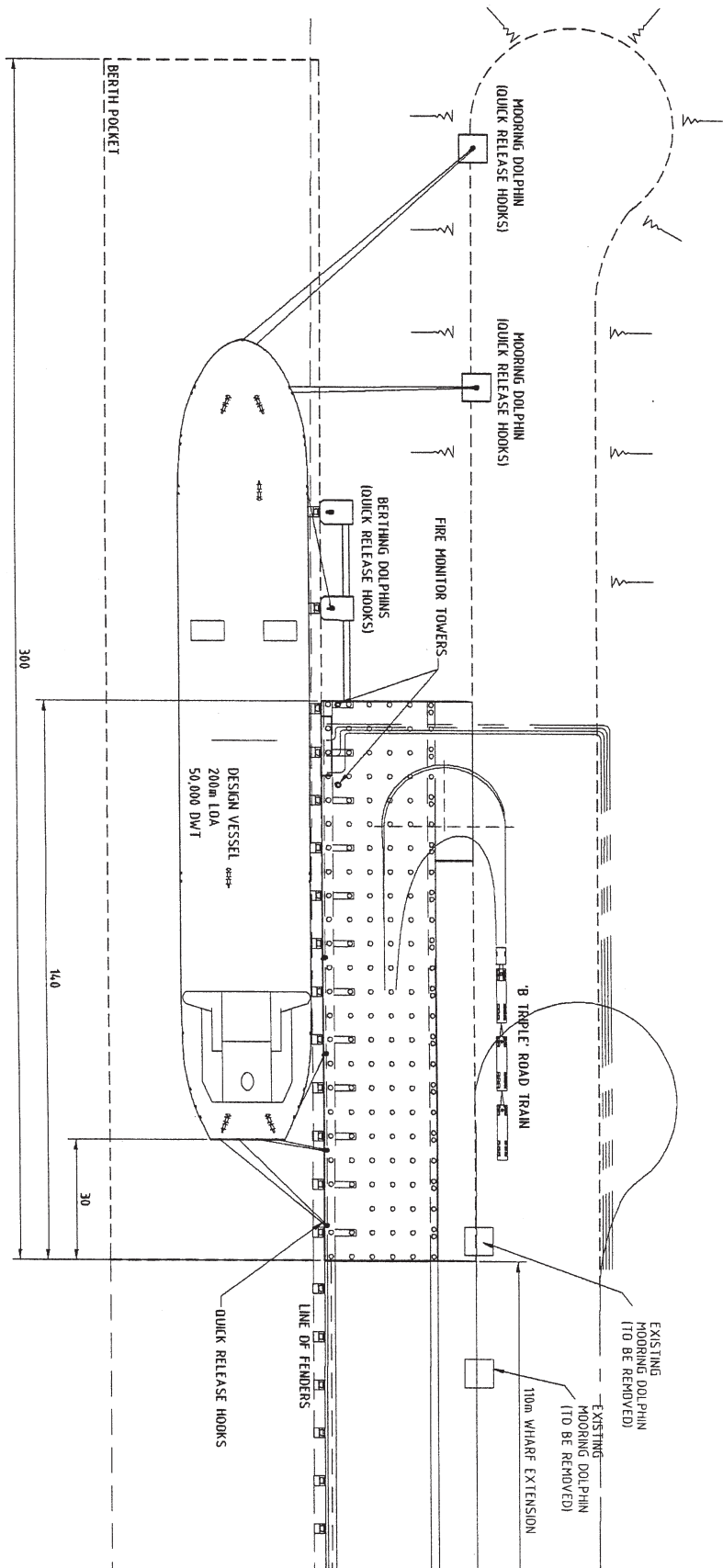
DETAIL A



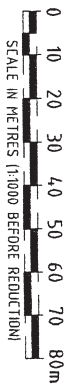
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EAST ARM PORT - STAGE 2
GENERAL ARRANGEMENT
DRG NO.: 101801-01



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PLAN

**EAST ARM PORT - STAGE 2
PROPOSED BULK LIQUIDS WHARF
DRG NO.: 1011801-02**

EAST ARM PORT DRAFT MASTERPLAN

KEY/LEGEND

- STAGE 1 - Existing Facilities.
- STAGE 2 - Intermodal container terminal.
- STAGE 2 - Multipurpose and bulk liquids.
- Future Facilities:
 - Future berth extensions
 - Bulk solids stockpile area
 - Marine industry support facilities
 - Future reclamation

